



Detroit Speed
Front Sway Bar Kit
1964-70 Ford Mustang & 1960-65 Ford Falcon/Mercury Comet
P/N: 031421DS & 031422DS

The Detroit Speed Front Tubular Sway Bar Kit is designed to improve cornering and reduce body roll on your early Mustang, Falcon, or Comet. DSE's larger than stock, 1-1/4" O.D. tubular sway bar includes a chassis spacer to improve travel on lowered vehicles. It is powder coated black and includes grease-able polyurethane bushings, low friction ball-style end links and all mounting hardware. The sway bar is compatible with factory and aftermarket control arms.



P/N: 031422DS

Item	Part Description	Quantity
1	1-1/4" O.D. Tubular Sway Bar	1
2	Polyurethane Sway Bar Bushing	2
3	Sway Bar Bushing Mounting Bracket	2
4	Sway Bar End Link Assembly	2
5	Sway Bar Chassis Spacer	2
6	1-1/4" Double Split Lock Collar Assembly	2
7	Sway Bar End Link Grommets	4
8	Sway Bar End Link Stamped Washers	4
9	Super Grease	1
10	Instructions	1

Hardware Kit Checklist – Detroit Speed Front Sway Bar Kit			
Part Number	Description	Quantity	Check
200135	Sway Bar Hardware Bag	1	
950056FS	3/8"-16 x 1" L Button Head Bolt	4	
980027FS	3/8"-16 x 1" L Hex Head Bolt	4	
960053FS	3/8"-16 Nylock Nut	10	
970023FS	3/8" SAE Washer	8	
960055FS	M12-1.75 Nylock Nut	2	
970026FS	M12 Washer	4	

Fastener Torque Specifications	
Application	Torque (ft.-lbs.)
Sway Bar Chassis Spacer	35
Sway Bar End Link	53
Split Lock Collar	15 (Blue Loctite)

Installation Instructions

1. To begin installation, chock the rear wheels. Raise the front of the vehicle and support the vehicle under the front control arms so the front suspension is at ride height.
2. Remove the existing sway bar from the vehicle.
3. Locate one of the sway bar chassis spacers and place two of the provided 3/8"-16 x 1" L hex bolts through the slots in the spacer. The hex head on the bolts will lock into the slots (Figure 1).

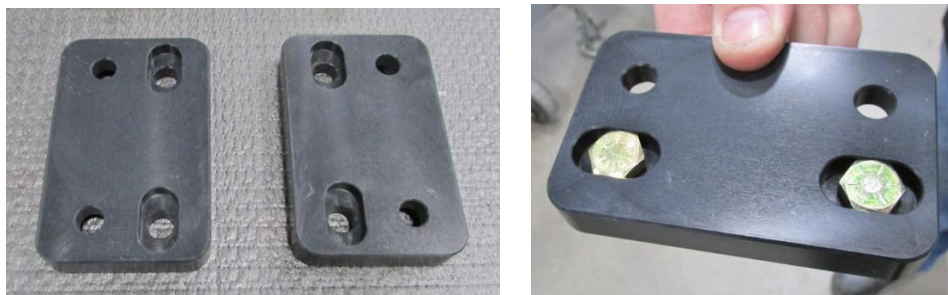


Figure 1 –Sway Bar Mount Spacer

4. Place the spacer with bolts to the bottom side of factory sway bar mount (Figure 2). Install the spacer assembly to the sway bar mount with the provided 3/8"-16 x 1" L button head bolts, washers and Nylock nuts.



Figure 2 –Sway Bar Mount Spacer Installed

5. Repeat Steps 3 and 4 for the opposite side of the vehicle. Torque the 3/8"-16 hardware to 35 ft.-lbs.
6. Install the urethane bushings on the sway bar using the provided Super Grease (Figure 3). Slide the sway bar bushing brackets over the bushings. **NOTE:** You can leave the plastic sleeve on the bar to protect the powder coat while installing the bar.



Figure 3 - Grease & Install Bushings

7. Position the DSE sway bar to the vehicle. Place the sway bar bushing brackets over the 3/8"-16 x 1" L hex head bolts that were installed in the spacers in Step 3.
8. Install the sway bar to the spacer bolts using the provided 3/8"-16 Nylock nuts (Figure 4). Center the sway bar in the vehicle and tighten the bolts. Do not torque at this time.

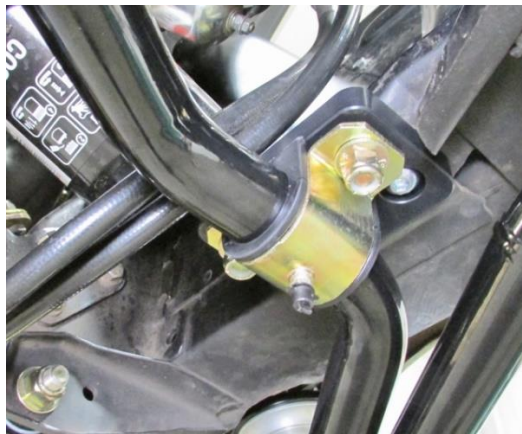


Figure 4 - Install Sway Bar Brackets

9. Locate the sway bar end links. Place one of the provided M12 washers over the threads of the ball joint stud. Place one of the stamped washers over the thread on the end link hex adapter followed by one of the grommets. Place this end of the end link through the sway bar mounting hole in the lower control arm (Figure 5).



Figure 5 - Locate End Links

10. Place another grommet over the threads of the end link and into the lower control arm mounting hole followed by another stamped washer. Thread the provided 3/8"-16 Nylock nut onto the threads and leave loose (Figure 6).



Figure 6 – Install End Link Hardware

11. Rotate the sway bar as needed and install the sway bar end link into the sway bar mounting hole. The stud will point toward the center of the vehicle. Place another M12 washer over the threads on the sway bar end link ball joint. (Figure 7). Repeat steps 9 through 11 for the opposite side of the vehicle.



Figure 7 – Install End Link to Sway Bar

12. Tighten both end links with the provided M12-1.75 Nylock nut and torque the hardware to 53 ft-lbs. (Figure 8).

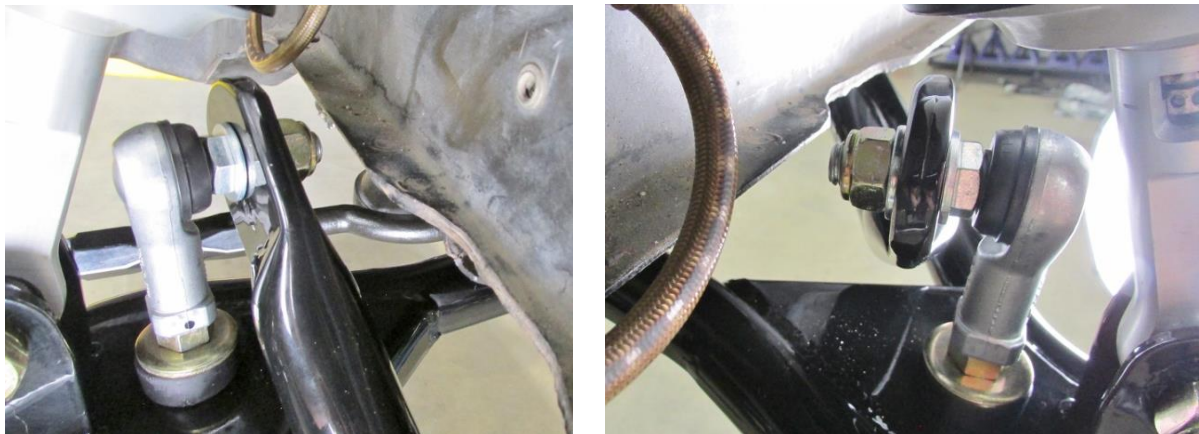


Figure 8 – Tighten End Link to Sway Bar

13. Tighten the 3/8"-16 Nylock nuts on both of the end links. Do not over tighten, the end links are tight when the polyurethane bushings start to compress.
14. Next, torque the 3/8"-16 fasteners that were installed in Step 8 in the sway bar bushing brackets to 35 ft-lbs. (Figure 9).



Figure 9 - Torque Bracket Hardware

15. Separate the split lock collars into two pieces and place them around the sway bar to the inside of the sway bar bushings.
16. Re-assemble the collars using medium strength blue Loctite 242 on the bolts and torque to 15 ft-lbs. **NOTE:** Position the collars tight to the urethane bushings when installing (Figure 10).

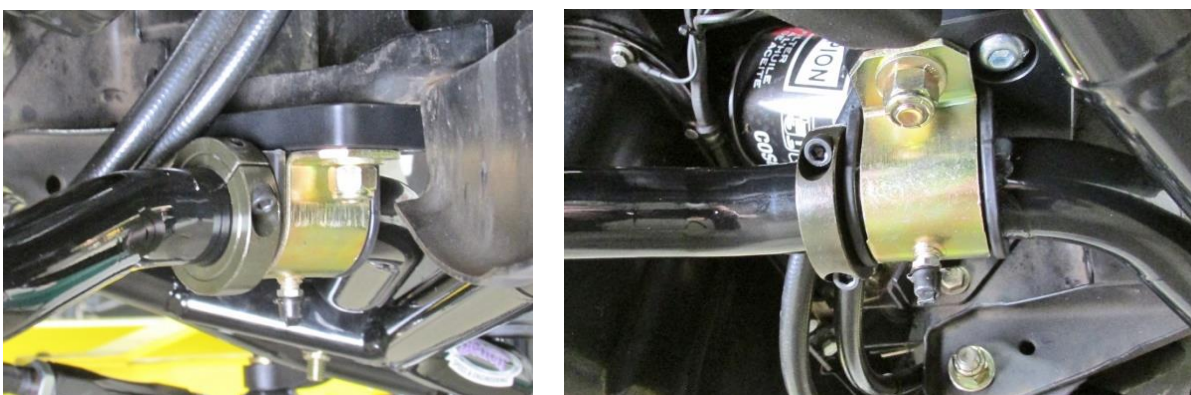


Figure 10 - Install Split Lock Collars

17. The installation is now complete.

1964-1965 Mustang Only:

NOTE: If your vehicle is equipped with the integral fuel pump/filter (Figure 11), the additional size of the sway bar may interfere with the filter portion. It is recommended to replace the fuel pump with a smaller profile pump.



Figure 11 - Integral Fuel Pump/Filter

See the selection of small block Ford Holley mechanical fuel pumps at holley.com:

P/N: 12-833 - 80 GPH Mechanical Fuel Pump

P/N: 12-289-11 - 110 GPH Mechanical Fuel Pump

P/N: 12-289-13 - 130+ GPH Mechanical Fuel Pump

P/N: 12-289-20 - 170+ GPH Mechanical Fuel Pump

If you have any questions before or during the installation of this product, please contact Detroit Speed at tech@detroitsspeed.com or 704.662.3272